



Metropolitan Transportation Authority

Capital Program Committee Meeting

January 2025

Committee Members

Janno Lieber, Chair
Meera Joshi, Vice Chair
Andrew Albert
Gerard Bringmann
Norman Brown
Samuel Chu*
Michael Fleischer
Dan Garodnick
Randy Glucksman
Marc Herbst
David Jones
Blanca Lopez*
David Mack*
Haeda Mihaltses*
John Ross Rizzo
John Samuelson
Vinnie Tessitore
Neal Zuckerman

Capital Program Committee Meeting

Monday, 1/27/2025
11:30 AM - 12:30 PM ET

1. SUMMARY OF ACTIONS- none this month

2. PUBLIC COMMENTS PERIOD

3. APPROVAL OF MINUTES

C&D CPC Committee Minutes - Page 3

4. 2025-2026 COMMITTEE WORK PLAN

C&D CPC Committee Work Plan - Page 9

5. PRESIDENT'S UPDATE

6. C&D INFRASTRUCTURE BUSINESS UNIT UPDATE

C&D Infrastructure Business Unit Report - Page 10

IEC Project Review on Far Rockaway - Page 14

IEC Project Review on 207 St Yard Flood Mitigation & Sewer Replacement - Page 19

IEC Project Review on Jamaica Bus Depot - Page 24

IEC Project Review on Bus Radio System - Page 30

7. C&D SAFETY REPORT

C&D CPC Safety Report - Page 36

8. CAPITAL PROGRAM STATUS REPORT

C&D Commitments, Completions, and Funding Report - Page 38

9. C&D PROCUREMENTS- none this month

MINUTES OF MEETING
MTA CAPITAL PROGRAM COMMITTEE
December 16, 2024
New York, New York
1:00 PM

CPC Members present:

Hon. Janno Lieber, Chair
Hon. Meera Joshi, Vice Chair
Hon. Andrew Albert
Hon. Gerard Bringmann
Hon. Samuel Chu
Hon. Randolph Glucksman
Hon. Marc Herbst
Hon. David Jones
Hon. Blanca López
Hon. David Mack
Hon. Haeda Mihaltses
Hon. John-Ross Rizzo

CPC Members not present:

Hon. Norman Brown
Hon. Michael Fleischer
Hon. Daniel Garodnick
Hon. John Samuelsen
Hon. Vincent Tessitore, Jr.
Hon. Neal Zuckerman

MTA staff present:

Evan Eisland
Joe Keane
Steven Loehr
Monica Murray
Mark Roche
Ziona Rubin
Cathy Sheridan
Jamie Torres-Springer
Anthony Tufano

Independent engineering consultant staff present:

Sirish Peyyeti

* * *

Chairman Lieber called the December 16, 2024, Capital Program Committee Meeting to order at 1:02 PM.

Public Comments Period

There were six public speakers during the hybrid public comment period: Joseph Morales, Kara Gurl, Omar Vera, Jason Anthony, Christopher Greif, Matty Bucky-Highland*

*Provided comment virtually.

CPC Work Plan

One change, the OMNY report is moved from May to April to be presented alongside the Systems business unit in 2025.

Upon a motion duly made and seconded, the Committee approved the changes to the work plan.

Meeting Minutes

Upon a motion duly made and seconded, the Committee approved the minutes of the meeting held on November 18, 2024.

Details of the following presentations, and Committee Members' comments and questions with respect thereto, are included in the video recording of the meeting produced by the MTA and maintained in the MTA's records.

President's Report

MTA Construction and Development Company ("C&D") President Torres-Springer provided an update on various ongoing and upcoming transportation projects. The Borough Hall Station ADA accessibility project is progressing well and is set to be completed by June. This logistically complex project, which involves the installation of three elevators while ensuring the structural integrity of the station's foundation, is on time and on budget. Similarly, the Queensboro Plaza station has been made fully ADA accessible, with two elevators installed. An event with Senator Gonzalez and Councilmember Won celebrated this achievement. This project was done in partnership with a developer, facilitated by zoning changes for accessibility in cooperation with the city. So far, 13 subway and railroad stations have been made accessible this year, with 36 more stations currently under construction to meet accessibility standards.

Congestion Pricing projects are moving forward. C&D has shortlisted firms for the Second Avenue Subway Phase 2 Contract 2, and the RFP will be issued this year with an award planned for 2025. The Fulton and Liberty Lines CBTC project is also advancing, with the Liberty Line to Ozone Park added to the scope expanding the CBTC coverage of that line. Additionally, procurements for ADA improvements at the Parsons Blvd, Briarwood, Gates Ave, and 42nd St Bryant Park stations are advancing, and Congestion Pricing will pay for 19 more stations and an additional 270 battery electric buses.

Looking ahead, Mr. Torres-Springer highlighted several key projects with procurement actions before the Board this month: ADA Package 6, which includes five ADA stations, including the newly added Middletown Rd station on the 6 train; the exercise of Option 2 under the R211 contract, which will add 445 more subway cars to the system; and the LIRR Dual Mode Locomotives project which will add up to 44 more locomotives to LIRR.

Based on the Boards approval of the 2025-209 Capital Plan in October, MTA C&D has held open houses in four of the five boroughs, and one is scheduled for tonight in the Bronx. Further open houses will be held across the region to gather public feedback and inform residents about the Capital Plan.

Mr. Torres-Springer announced that MTA had reached an important milestone on the OMNY contract with the completed rollout of reduced fare cards.

Finally, President Torres-Springer thanked Senior Vice President, Bridges & Tunnels (“B&T”) Business Unit Joe Keane, for his leadership since 1988.

Railroads Business Unit

Anthony Tufano, Senior Vice President, Railroad Business Unit, began his presentation with an overview of the current and upcoming Capital Programs. He noted that the current program allocates \$7 billion to the Railroads Business Unit, including \$6 billion for construction projects, while the next program will increase significantly to \$14 billion, with \$11 billion earmarked for construction. The focus of the upcoming program will remain on maintaining a state of good repair, with major investments in stations, line structures, and power systems. Highlights include \$1.4 billion for seven new ADA-compliant stations and 11 full reconstructions, \$1.7 billion for continued improvements to Grand Central’s infrastructure, over 40 bridge paintings, and 10 full bridge replacements. Additional emphasis will be placed on slope stability and resiliency, particularly along the Hudson Line, and a significant power investment involving the upgrade of more than 10 substations.

Mr. Tufano emphasized the importance of team development to prepare for the expanded program, highlighting efforts to train, unite, and grow teams across the two railroads for efficient project delivery. He outlined a strategy to bundle projects into programs to maximize efficiency, such as integrating the Grand Central Terminal Train Shed and Park Avenue Viaduct projects under a single management team. These efforts aim to accelerate timelines and optimize resources, with similar approaches planned for line structures and station upgrades.

Moving into project updates, Mr. Tufano reported that the Railroads Business Unit had committed \$831 million in projects this year, with \$168 million in procurement, and completed \$373 million worth of projects to date, with an additional \$227 million expected by year-end. He provided updates on several key projects, starting with the Grand Central Train Shed Sector 1, which is trending \$20 million under budget and on schedule. The project involves replacing steel structures and utilities under streets, with early completion of 47th Street achieved before Thanksgiving. The success of Sector 1 has led to plans for a larger replacement in the next capital program, reducing the overall timeline from 35 years to 15 years.

Next, Mr. Tufano highlighted the Park Avenue Viaduct project as a model for the agency, with significant progress made using innovative method. Ten weekends of span replacements were completed, replacing eight city blocks of the viaduct. By executing Phase 2 work concurrently with Phase 1, the project will save 51 months on the original schedule.

On the LIRR side, ADA Package 1, involving nine stations, is on schedule and on budget, with four stations—Amityville, Copiague, Lindenhurst, and Merrick—successfully completed and well-received by the community. The remaining stations are under construction and expected to be completed by year-end, bringing the total number of ADA-compliant stations on the LIRR to 119. Plans are in place to achieve 100% ADA compliance in the next capital program. Finally, Mr. Tufano discussed the Jamaica Capacity Improvements program, specifically the Hall Interlocking project, which aims to reduce congestion east of Jamaica by constructing a 280-ton bridge and creating new routes. This project is progressing on schedule and budget, with bridge installation planned for early 2025.

IEC

Sirish Peyyeti, Program Director of the IEC, presented a review of five railroad projects, including three for LIRR and two for MNR. He noted that all reviewed projects are currently on budget and on schedule. The project teams have successfully identified and mitigated risks, such as workforce coordination, long lead items, and competing projects, ensuring minimal scheduling conflicts and the availability of resources for construction activities.

For the Grand Central Terminal Train Shed Rehabilitation (Sector 1), the project is 68% complete and ahead of schedule. The Park Avenue Viaduct Replacement Project has made significant progress, with Phase 1 nearing interim completion in April 2026, and Phase 2 is forecasted to achieve substantial completion by September 2027. Progress in Phase 1 has enabled the team to advance Phase 2 work, which is critical to maintaining structural integrity and minimizing disruptions to surrounding communities.

On the LIRR side, ADA Package 1, which provides accessibility improvements at nine stations, is progressing well, with four stations achieving beneficial use since the last report. Upon completion, the package will bring the total number of accessible stations to 119, or approximately 97% of the system. For ADA Package 2, which is 16% complete, construction at Babylon Station is on schedule, but a temporary construction pause is expected to impact overall project cost and schedule until its anticipated lift in 2025. The Jamaica Capacity Improvements Phase 2 – Hall Interlocking project is 40% complete, with substantial completion forecasted for October 2026.

Bridges and Tunnels Business Unit

Joe Keane, Senior Vice President, Bridges & Tunnels Business Unit, provided an update highlighting the division's achievements and future plans. Bridges & Tunnels operates seven bridges and two tunnels across New York City, serving approximately 325 million vehicles annually. With facilities ranging in age from 54 to 88 years, the division emphasizes sustained investment in structural integrity, safety, and maintaining a state of good repair.

In 2024, B&T's core capital program was allocated \$2.8 billion, with planned commitments of \$686 million. Although many projects faced delays due to congestion pricing holds, all procurements are now moving forward for award in 2025. B&T exceeded its project completion goals, delivering \$770 million in completed projects—25% above target. The 2025 Capital Program, totaling \$3 billion, focuses on critical maintenance, safety enhancements, and extending the service life of key infrastructure. Significant initiatives include the dehumidification of main cables at the Bronx-Whitestone and Throgs Neck bridges, installation of enforcement-grade weigh-in-motion systems, and fixed fire suppression systems at tunnels. The program also invests in the resilience of electrical systems at multiple facilities.

Accessibility improvements remain a priority under the MTA's Strategic Action Plan. Major upgrades at the RFK, Cross Bay, and Henry Hudson bridges have improved bike and pedestrian access, with many projects completed ahead of schedule. At the RFK Bridge, reconfigured vehicular access and new ADA-compliant ramps were delivered six months early and \$5 million under budget. Similarly, the Cross Bay Bridge saw the completion of a contract that included structural rehabilitation of the bridge and construction of an ADA-compliant bike and pedestrian ramp. This rehabilitation project deferred a full span replacement, avoiding \$300 million in costs.. At the Henry Hudson Bridge, widened sidewalks, seismic retrofits, and a new bike/pedestrian ramp were completed on time and \$4 million under budget.

B&T continues to prioritize resilience projects, particularly protecting its bridges against ship collisions. Robust collision protection systems have been installed or upgraded at all navigable waterways, including the Bronx-Whitestone Bridge in 2021. At the Throgs Neck Bridge, ongoing

projects bundle structural upgrades, painting, and cable replacements, and are currently 35% complete. Future projects at the Verrazzano-Narrows Bridge include reconstructing the upper-level Brooklyn ramps and dehumidifying the main cables. These projects, totaling \$800 million, are planned for award in 2025.

IEC

Mr. Peyyeti presented the quarterly review of the RFK Bridge Structural Rehabilitation Project and the Traffic Light Report. He noted that year-to-date, B&T projects in the Traffic Light Report have performed well, with positive outcomes in managing costs and schedules. This was the IEC's first report to the CPC regarding the RFK Bridge Project, which was awarded in December 2023. The IEC concurred with B&T's assessment of the project's cost and schedule. Looking ahead to 2025, the IEC plans to monitor additional B&T projects and provide reports to the committee.

For the third quarter of 2024, the IEC reviewed 14 B&T projects for cost and schedule adherence, finding no variances during the reporting period. Across all 261 projects included in the TLR, 90% did not trigger any variances. Among the remaining projects, 8% experienced schedule issues, and 2% had cost or combined cost and schedule issues. These results reflect an overall positive trend in project performance.

The IEC attributed this success to several key strategies: proactive collaboration with third-party stakeholders and MTA Force Account groups to ensure planned outages, early identification of conflicts with internal and external projects, and securing adequate support for upcoming work through MTA operations.

Looking forward, the IEC identified opportunities to improve project outcomes in areas such as software and technology, track access, communications, signals, rolling stock, and enhancing collaboration between C&D Stations, BUU, and MTA planning teams. These efforts are critical for meeting the growing demand for accessibility projects under the new capital program.

Procurement Actions

Evan Eisland, Executive Vice President and General Counsel, C&D, presented ten procurement actions to the Capital Program Committee.

Upon a motion duly made and seconded, the Capital Program Committee voted to bring the following procurement actions before the full MTA Board and recommended the following:

- 1-2. Award of a publicly advertised and competitively solicited contract with MLJTC2, a joint venture between MLJ Contracting Corp. and TC Electric, LLC (Contract No. A37806), for Design-Build services for ADA Upgrades – Package 6, which provides for accessibility improvements at five New York City Transit stations and award of a long-term elevator maintenance contract;
3. Award of a publicly advertised and competitively solicited contract with BEI Holdings, Inc. (Contract No. 1000133778R) for Design-build services to upgrade the hardware and software for Metro North Railroad's Private Branch Exchange system;
4. Award of a publicly advertised and competitively solicited contract with ExterNetworks, Inc. (Contract No. 1000187450) for Design-Build services for Radio System upgrades – Phase 1 to replace Metro North's existing radio dispatch system;

5. Award of a publicly advertised and competitively solicited contract with TUV Rheinland of North America, Inc. (Contract No. D81491) for an independent safety assessor for the signal system modernization work at New York City Transit's Beach 105th Street Station interlocking;
6. Ratification of a modification to a contract with Walsh Construction Company II, LLC (Contract No. C34838) for accessibility upgrades to the Relay Building A site at New York City Transit's 207th Street Yard to be compliant with the Americans with Disabilities Act requirements;
7. Ratification of a modification to a contract with Walsh Construction Company II, LLC (Contract No. C34838) to re-route water and sewer lines to address differing site conditions and water pressure issues for Relay Building A at New York City Transit's 207th Street Yard;
8. Ratification of a modification to a contract with FOS Development Corp. (Contract No. C48703) for additional structural steel repairs to forty-one roof beams and five columns on Tracks E1 and E4 in the vicinity of New York City Transit's Bergen Street Station;
9. Ratification of a modification to a contract with John Civetta & Sons, Inc. (Contract No. 1000135846) to construct a dedicated exhaust system for the Unified Trash Facility at Grand Central Terminal; and
10. Ratification of a modification to a contract with Atkins-HNTB, JV, a joint venture between Atkins North America Inc. and HNTB New York Engineering and Architecture, P.C., (Contract No. PS21002) for additional services in support of the implementation of Communications Based Train Control for New York City Transit and extending the period of performance by 12 months.

Refer to the staff summaries and documentation filed with the records of this meeting for the details of these items, and refer to the video recording of the meeting, produced by the MTA and maintained in MTA records, for Board members' and C&D representatives' comments.

Adjournment

Upon motion duly made and seconded, Chairman Lieber adjourned the December 16, 2024, Capital Program Committee Meeting at 1:57 PM.

Respectfully submitted,
Lizzy Berryman
MTA C&D

2025 Capital Program Committee Work Plan

I. Recurring Agenda Items

Approval of the Minutes
Committee Work Plan
Commitments/Completions and Funding Report

II. Specific Agenda Items

February 2025

President's Update
Agency Initiatives

March 2025

President's Update
Signals
Quarterly Traffic Light Report

April 2025

President's Update
Systems & OMNY

May 2025

President's Update
Agency Initiatives

June 2025

President's Update
Rolling Stock
Diversity
Quarterly Traffic Light Report

July 2025

President's Update
Integrated Projects

September 2025

President's Update
Agency Initiatives
Quarterly Traffic Light Report

October 2025

President's Update
Stations

November 2025

President's Update
Railroads

December 2025

President's Update
Bridges & Tunnels
Quarterly Traffic Light Report

January 2026

President's Update
Infrastructure

MTA Capital Program Committee Update

Infrastructure Projects

January 2025

MTA Construction & Development's (C&D) last report to the Capital Program Committee for New York City Transit (NYCT) Infrastructure projects was in January 2024. The Infrastructure Business Unit (BU) within C&D focuses on maintaining a state of good repair for NYCT infrastructure, as well as bolstering the resiliency and sustainability of the transit system. Our projects span four primary program areas within the Infrastructure BU: Line Structures, Facilities and Yards, Wayside Power, and Superstorm Sandy Recovery & Resiliency.

The Infrastructure BU awarded approximately \$1.16 billion in new projects in 2024. This included 5 Depots: Clara Hill, Grand Av, Gun Hill, Kingsbridge, Queens Village (\$196.2M), Overcoating: Portal to Kings Highway/Culver (\$138.9M), Substation Renewal Hester Street and Village (\$86.1M), and Overcoating: 225th Street to 240th Street / Broadway to 7th Avenue (\$120M).

The Infrastructure BU achieved substantial completion on 48 projects in 2024, valued at approximately \$1.72 billion. These include Mainline Track – Direct Fixation Jamaica LL and 63rd Street (\$164.8M), Tiffany Central Warehouse (\$58.4M), 207th Street Yard (\$563M), and Bus Radio (\$316.3M).

Anticipated commitments in 2025 include Rehabilitation of 5 Circuit Breaker Houses (\$89.5M), Westchester Yard (\$120.6M), and Overcoating: West End (\$292.6M).

Key anticipated completions in 2025 include Rail Car Acceptance and Testing Facility (\$116.2M), Paint/Structure Repair: King’s Highway – West 8th Street / Culver (\$99M), 2 new substations (Canal Street / 8th Avenue and 28th Street / 8 Avenue \$146.1M, and Battery Electric Bus Charging Infrastructure Phase 2 – 6 Depots (\$196.2M).

207th Street Yard Flood Mitigation and Sewer Replacement

NYCT’s 207th St Yard is the main storage yard for the rolling stock on the A and C lines and includes the maintenance shop for the A B C D lines and the overhaul facility for the 1-7 lines. Like the Coney Island Yard, in 2012 Superstorm Sandy storm surge flooded the 43-acre 207th St Yard and the site needed critical repairs to its rail operation and additional flood protection.

The 207th Street yard is the second largest yard in the Subway System behind the Coney Island yard. All construction work was conducted while keeping all 62 tracks in the yard in full operation.

This project reached Substantial Completion in 2024 and all protections against future storms are in place.

The primary project components included:

- 2100’ of flood walls and 5 flood gates surrounding the facility
- 2 state-of-the-art signal buildings including RTO tower
- Replaced 80% of yard tracks and installed a new signal system
- Interceptor sewer outside of NYCT’s 207th Street Yard to replace the existing sewer system within the yard
- Signal trays – lubrication system
- Upgraded all traction power controls and distribution to 3rd rail
- Installed a new water main line to serve Building A and fire suppression system/equipment

Project Status: Substantial Completion and Budget

Project	Previous	Forecast	Percentage Complete
Yard Flood Mitigation & Signal Repairs	Nov 2023 \$637M	May 2024 \$649M	100%
Sewer Replacement	June 2024 \$163M	November 2024 \$152M	100%
Total	\$800M	\$801M	

The project is now performing punch list work due to be completed in April 2025.

Bus Radio System

The new bus radio system was declared Substantially Complete on 12/31/2024. This system provided approximately 6,000 buses and non-revenue vehicles with a new radio communication system replacing a system more than 30 years old. The new system provides reliable voice and data communications in real-time enhancing data collection and overall bus performance. The project included:

- 35 base radio stations in the Bronx, Brooklyn, Manhattan, Queens, Staten Island, New Jersey, and Yonkers
- New Bus Command Center outfitted with new hardware and software for Computer Aided Dispatch of the entire bus fleet and non-revenue bus support vehicles

Completion of final punch list work is underway.

PROJECT STATUS	Previous	Forecast
Budget	\$330M	\$356M
The project is more than 96% complete.		

Rockaway Line Resiliency & Viaduct Rehabilitation

The Rockaway Line, which crosses the Jamaica Bay estuary, was severely damaged by Superstorm Sandy resulting in a full suspension of its service for seven months while critical repairs were conducted to bring service back. The purpose of this new long-term flood mitigation project is to further protect critical assets along the Rockaway Line and allow service to quickly resume following any future storm surge events. The mitigation work is bundled with critical state-of-good-repair work, including viaduct rehabilitation and upgrades to the South Chanel Bridge.

The Rockaway Line “A” train provides service between the north tip of Manhattan, through Brooklyn to the Rockaway community and its beaches.

Overall highlights of the project include:

- Hammels Wye Viaduct Demolition and Replacement
 Imminent focus is on the 17-week service shutdown that is occurring between 1/18/2025 to 5/19/2025. C&D has begun the demolition of the 1,440-foot elevated viaduct.
- South Channel Bridge Repairs
 Concurrently, we are rehabilitating the South Channel Bridge, where all the controls and mechanicals are being replaced to assure a safe and reliable operation service.

- Rockaway Line Viaduct Rehabilitation

The Rockaway Line Viaduct consists of approximately 5 miles of elevated structure on the Rockaway Peninsula from Beach 116th St-Rockaway Park to Far Rockaway-Mott Avenue. This rehabilitation work will include steel and concrete repairs as well as wrapping the concrete beams with Fiber Reinforced Polymer (FRP).

PROJECT STATUS	Current	Forecast
Substantial Completion	Aug 2026	Aug 2026
Budget	\$599 M	\$599 M
The project is 29% complete.		

The project is on schedule and on budget with all efforts currently focused on works within the 17-week outage.

Jamaica Bus Depot

This project is for design and construction of a state-of-the-art bus maintenance and storage facility to replace the existing Jamaica Depot. The current building was opened in 1939. The new LEED certified depot will have a maintenance building with rooftop parking, an outdoor parking lot, and an administration building tied to the depot with a bridge. The facility will have 272 bus parking spots, 15 maintenance bays, 3 drive-through washes, 3 fueling stations, and three chassis/interior washes. Upon completion the depot will be able to charge 60 electric buses, but the entire depot is designed to accommodate an all-electric fleet projected to be completed under a separate contract.

PROJECT STATUS	Current	Forecast
Substantial Completion	Dec 2026	Oct 2027
Budget	\$654 M	\$660 M
The project is 27% complete.		

The project was awarded as a Design-Build contract at the end of 2022.

Design Update:

- Design nearing completion. 8 of 12 design units released for construction, and 4 design units at 100%.
- A feasibility study is currently being explored to increase the electric bus capacity using high tension power supply.

Construction Update:

- York parking lot completed and placed in service in May 2024.
- Adjacent commercial properties acquired and demolished.
- Sheet piling completed for ConEd vault, underground storage tanks and bulk fluid storage.
- ConEd vault for depot electrical service: 90% completed.
- Storm water detention system: 50% completed.
- Piles for the administration building and the new depot completed.
- Foundations (pile caps and grade beams) in progress: 36% completed.

- Steel installation in progress.
- Temporary Residential easement: Relocation process for affected properties is underway.

The current date for substantial completion of the project is running late as a result of third party and design development. The team is exploring alternative mitigation strategies to return the project to the original completion date.

January 2025 CPC Independent Engineering Consultant Project Review

BU: Infrastructure C-35327 Design-Build Services Rockaway Line Resiliency & Viaduct Rehabilitation



Michael Baker | MTA Independent Engineering Consultant
INTERNATIONAL

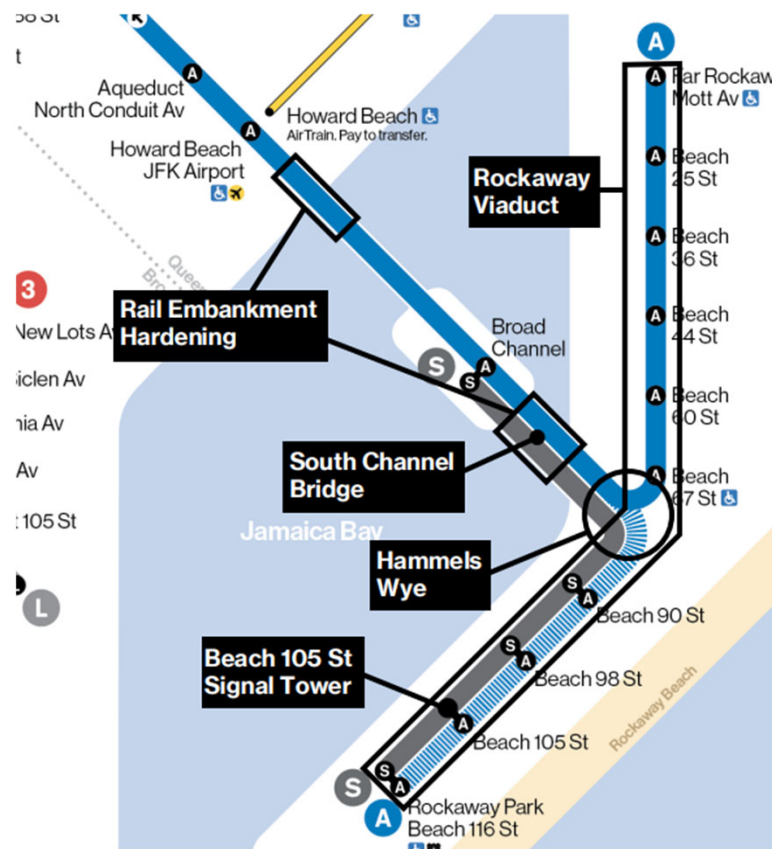
C-35327 Rockaway Line Resiliency & Viaduct Rehabilitation

Scope

The Rockaway Line, originally built by the Long Island Rail Road in 1892, has evolved into a critical transit link connecting the Rockaway peninsula to New York City. After a fire in 1950, the City repurposed the line for subway use, but it faced service challenges until its vital role was solidified.

Today, the line serves over 9,000 daily riders, with significant investments ensuring its functionality and resilience for future generations.

The ongoing rehabilitation and resiliency efforts aim to safeguard the line's infrastructure against future environmental risks. Key project elements include the reconstruction of **Hammels Wye**, rehabilitation of the **South Channel Bridge**, **viaduct concrete repairs**, and **embankment fortifications** as well as adding a **signal tower** to Beach 105th Street in Rockaway Park.



C-35327 Rockaway Line Resiliency & Viaduct Rehabilitation

Schedule

- The Design-Build contract was awarded in December 2022 for a construction duration of 44 months.
 - Milestone 1: Complete Viaduct Rehabilitation at the Hammels Wye within 16 weeks, yet no later than May 19th, 2025
 - Milestone 2: Rehabilitation of the South Channel Bridge 869 Days after NTP yet no later than May 19th, 2025
 - Contractual Substantial Completion (SC) – 1339 Days after NTP, August 31st, 2026
- The contract duration is approximately 53% complete.
- Long-term General Order (GO) pre-coordination meetings with all stakeholders are ongoing to ensure all planned work can be completed and the GO can be returned to revenue service within the scheduled timeframe.

Budget

- Current Project Budget: \$599M
 - Design-Build contract: \$393M
 - Other MTA cost: \$206M
- The IEC notes that the expenditures to date are in line with project progress
- Estimate at Completion (EAC): \$599M
 - The project's budget allocations are sufficient at the project current phase

C-35327 Rockaway Line Resiliency & Viaduct Rehabilitation

Risks and Mitigations

- Completion of Hammels Wye and South Channel Bridge by Memorial Day 2025
 - Pre-delivery material inspections by Designer of Record and Quality Managers
 - Weekly tracking of steel and concrete fabrication progress
 - Expedited design/submittal/approval of long-lead items
 - Addressing Factory Acceptance Testing issues, scheduling retests, and monitoring special parts/controls for South Channel Bridge
- Severe Weather Conditions
 - Starting one week early to extend diversion duration
 - Including one weather day per week in the schedule
- Material Availability and Supply Chain Issues
 - Tracking long-lead items and using multiple suppliers
 - Prioritizing submittal review and approvals
- West Branch Viaduct Rehabilitation - added concrete repairs
 - Monitoring repair progress and Notice of Change evaluations
 - Refining repair details for efficient field execution
- Lack of External Agency Permit Coordination
 - Continued Active engagement with third parties, utilities, and agencies (e.g., USACE, USCG, PANYNJ)
- Limited MTA Support
 - Early submission of requests
 - Intensive coordination with Support Services to align activities

The IEC notes that the Project Management Team's efforts to coordinate the design and construction processes and mitigate the project's risks are appropriate.

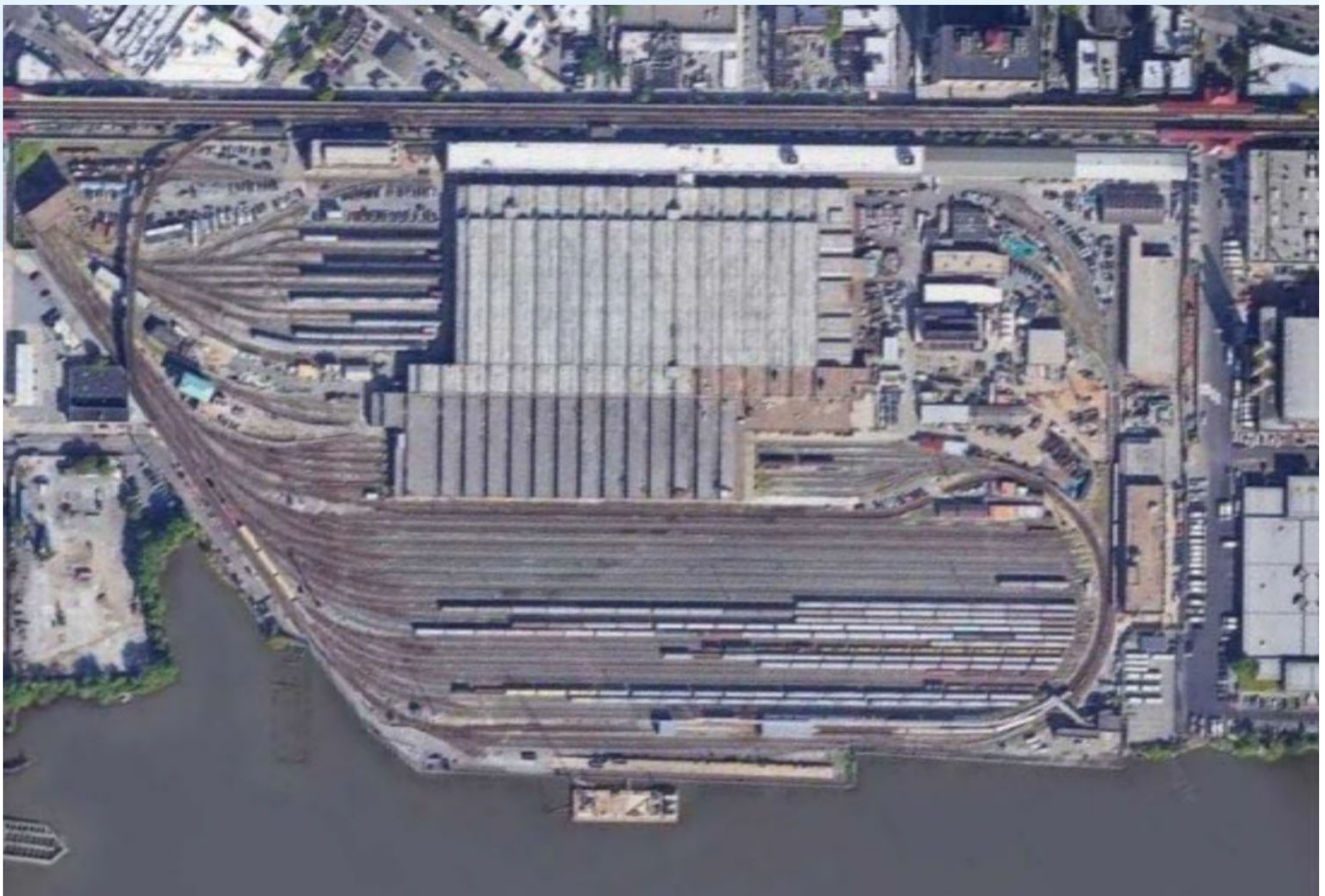
C-35327 Rockaway Line Resiliency & Viaduct Rehabilitation

IEC Observations

- Successfully prepared for and commenced the long-term General Order. The diversion spans approximately 4 months and coincides with off-peak travel seasons to minimize commuter disruptions.
- The long-term diversion will allow structural repairs, track replacement, signal and electrical work, etc. to be performed with efficient, purpose-built equipment as well as prefabricated components.
- The IEC notes that the project team has halted track, signal and communications scope at Beach 105th Street, while it undergoes technical evaluation.
- The entire Project Management Team is keeping external stakeholders (e.g., DOT, Coast Guard, PANYNJ) updated on design and construction progress to ensure timely support and facilitating interactions with affected entities.
- The project's community outreach team demonstrates a proactive approach to minimizing construction-related impacts on the public. Throughout the project, the team ensures timely updates on neighborhood-impacting changes.
- The project is exceeding federal local hiring goals, introduced in March 2022, with the Design Build Team achieving 33.65% local hires against a 20% target.
- The project is on track to meet the Disadvantaged Business Enterprise (DBE) goals, which foster opportunities for minority- and women-owned businesses.

January 2025 CPC Independent Engineering Consultant Project Review

BU: Infrastructure New York City Transit – 207th Street Yard Flood Mitigation & Sewer Replacement



Michael Baker
INTERNATIONAL

MTA Independent Engineering Consultant

NYCT – 207 St Yard Flood Mitigation & Sewer Replacement

Scope

207th Street Yard, which comprises of a rolling stock storage yard, maintenance shop, and overhaul facility sustained extensive water damage during Super Storm Sandy from storm surge of the Harlem River and sewer backflow flooding. The 207th St program includes several projects of which two have been monitored by the IEC:

- Yard Flood Protection and Signal Repairs
 - Installation of a perimeter protection wall and deployable gates to protect the yard and portal from future tidal surges
 - Construction of two new Relay Buildings, five new Central Instrumentation Houses and one enclosure to house new equipment for signals, equipment racks and auxiliary systems
 - Design, furnish, install, test and place in-service new signal system and auxiliary support systems throughout the 207th Street Yard, and modify interfaces with the main line
 - Repair/replace track and interlocking system damaged by Super Storm Sandy
 - Replace yard traction power components

- Sewer Replacement
 - Relocation of the existing New York City Department of Environmental Protection (NYCDEP) sewer main, regulators, diversion chambers, tide gates, and internal sanitary service connections in the yard
 - Construction of new pump stations to service yard facilities
 - Installation of reinforced sewer lines and backflow prevention valves

NYCT – 207 St Yard Flood Mitigation & Sewer Replacement

Schedule

Both projects have reached Substantial Completion in Q4, 2024, with all major elements completed and in service, punch list items remain.

- Yard Flood Protection and Signal Repairs
 - The project duration has been extended as additional work was incorporated due to field conditions and design changes. Further 7 months delays incurred since the previous report.
 - An initial time extension was granted for some delays and negotiations are ongoing for a supplementary time extension.

- Sewer Replacement
 - The project duration has been extended due to public utilities work and private property access issues. While mitigation efforts resulted in partial schedule recovery, a time extension was negotiated.
 - The project completed within the granted extension, with no further delay since the last report.

NYCT – 207 St Yard Flood Mitigation & Sewer Replacement

Budget

Yard Flood Protection and Signal Repairs

	Project Budget	C&D Estimate at Completion	IEC Estimate at Completion
At Award	\$633.5M	\$633M	\$633M
Current Status	\$648M	\$648M	\$648M

- Added work, schedule delays, and increased consultant and in-house support cause overall budget overruns.
- The IEC Estimate at Completion include cost exposure for remaining commercial issues.

Sewer Replacement

	Project Budget	C&D Estimate at Completion	IEC Estimate at Completion
At Award	\$152M	\$152M	\$152M
Current Status	\$144M	\$144M	\$144M

- While budget overruns were incurred due to the extended project duration, the project is forecast to complete within the original budget.

A final budget modification is in process to reconcile all project costs for both projects.

NYCT – 207 St Yard Flood Mitigation & Sewer Replacement

Observations

- ▣ The 207 St Yard flood mitigation and sewer replacement project completion result in the yard being protected from future storm events as designed.
- ▣ The repaired signal system and new relay towers are in use. Occupancy of the relay towers is forthcoming.
- ▣ The sewer project has been efficiently managed to complete the project within the original budget in light of the significant coordination challenges that resulted in delays.

January 2025 CPC Independent Engineering Consultant Project Review

BU: Infrastructure New York City Transit – Jamaica Bus Depot



Michael Baker Engineering, Inc.

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NYCT – Jamaica Bus Depot

Scope

The 83-year-old existing Jamaica Bus Depot, used for NYCT buses parking, maintenance, refueling, and washing has exceeded its useful life and needs replacement. The existing depot will remain in continuous operation, while a temporary bus parking lot will allow the same level of service to be maintained, during the phased construction of a new facility at the existing location

- The new facility will include a 134,000 SF bus depot, an outdoor bus parking area, and a 37,000 SF three story administration building
- Facilities for 60 Battery Electric Bus (BEB) charging are included, in addition, infrastructure for the expansion to 100% zero emission all electric bus fleet is incorporated
- The depot reconstruction is delivered as a Design-Build contract. It includes construction of a temporary bus parking lot, previously designed under a separate contract



Michael Baker Engineering, Inc.

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NYCT – Jamaica Bus Depot

Schedule

- The Design-Build contract was awarded in December 2022. Construction duration was set to 48 months
 - One contractual interim milestone is the completion of the new outdoor bus parking lot – July 2026
 - Contractual Substantial Completion (SC) – December 2026
 - MTA SC forecast – July 2027
- Construction at York temporary parking lot has been completed, the lot has been occupied and is being used by the Department of Buses
- Temporary fueling system has been commissioned and is in use, existing fueling system and all other structures on the future depot site have been removed
- Future depot and administration building foundation system installation is ongoing
- Contract work is 25% complete, contract duration elapsed – 50%, inline with expectations for a design-build project
- DEP permit delays impact the project schedule, two months further delay than the previous report
- The IEC SC forecast is October 2027, several schedule improvement measures are being evaluated, yet full recovery is unlikely
- Selected major milestones (not contractual) are shown below:

	Stage	Major Milestone	Baseline	IEC Forecast	Variance (Months)
1	1	York Lot Construction Start	Mar-23	Nov-23 (A)	-8
2		York Lot Completion	Oct-23	May-24 (A)	-7
3	2	New Bus Depot Maintenance Shop Building Envelope Completion	Dec-24	Oct-25	-10
4		New Bus Depot Permanent Power Connection	May-25	Mar-26	-10
5		New Bus Depot Maintenance Shop Completion	Sep-25	Jun-26	-9
6	3	Existing Bus Depot Maintenance Shop Demolition	Dec-25	Oct-26	-10
7	4	New Outdoor Parking Lot Completion (Contract Milestone 1)	Jul-26	Mar-27	-8
8		New Administration Building Completion	Dec-26	Oct-27	-10
9		Substantial Completion	Dec-26	Oct-27	-10

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NYCT – Jamaica Bus Depot

Budget

	Project Budget	C&D Estimate at Completion	IEC Estimate at Completion
At Award	\$626M	\$626M	\$626M
Current Status	\$626M	\$626M	\$626M

- Project Budget includes:
 - Design-Build contract: \$480M
 - Other MTA cost: \$146M
- Total expended to date is \$147M
- Real Estate Acquisition budget is not included in the summary above
- The IEC performed a review of the total project cost, inclusive of potential change orders and project risk. We conclude that although some soft costs overruns are projected due to schedule delays, the project budget is sufficient for completion

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NYCT – Jamaica Bus Depot

Risks and Mitigations

- Schedule impacts due to delays in testing and commissioning throughout the project phasing
 - The project phasing requires multiple testing and commissioning iterations to advance, for which early plans are being developed and stakeholder engaged to streamline the process
- Schedule impacts due to delays in hazardous material abatement in the existing depot
 - To reduce potential coordination issues, transferring the abatement scope to the design-builder (typically completed by a separate MTA contractor) is being evaluated
- Schedule impacts due to delays in BEB chargers procurement
 - Chargers are to be procured via NYPA on a separate MTA Contract. Chargers vendor has been selected, but procurement has yet to be finalized. The project management team (PMT) is closely monitoring the procurement progress and investigating alternatives to mitigate project impacts

The IEC finds the MTA Project Management Team (PMT) comprised of MTA C&D and the PMC, is effectively mitigating the noted risks and it's the IEC's opinion that the mitigation strategies are reasonable

Michael Baker Engineering, Inc.

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NYCT – Jamaica Bus Depot

Observations

- An analysis of all project delays is ongoing by the design-builder and the PMT to reestablish the contract schedule
- Close coordination with MTA Department of Buses continues to ensure the future facility is designed and constructed with consideration to operation ease and efficiency
- While permitting delays impact the project schedule, the PMT efforts mitigated further delays in expediting real estate acquisitions
- The project team effectively coordinates community outreach efforts to minimize impacts to the public while expediting construction progress

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January 2025 CPC Independent Engineering Consultant Project Review

BU: Infrastructure MTA C&D Bus Radio System

Bus Radio System

Scope of Work

- Bus Radio System (BRS) is a Design Build project that replaces the NYCT and MTA Bus analog bus radio system. The BRS contract award amount of \$202M was issued to Parsons Transportation Group, with Notice to Proceed in March 2016 and Substantial Completion (SC) date of January 2021.
- BRS is a fully integrated digital bus radio system that provides for voice and data communications for all NYCT and MTA Bus operators and field supervisors.
- Project includes:
 - Installation of 35 base stations throughout the five boroughs, Yonkers and Kearny, New Jersey
 - Outfitting approximately 6,000 buses and 92 non-revenue vehicles with new radio equipment.
 - Providing 1250 portable radios.
 - Outfitting the new Bus Command Center (BCC) with bus dispatch consoles and radio equipment hardware and software to support voice and data communications between the BCC and individual or groups of buses.
 - Full integration and testing of all base stations, buses, and the BRS network.

Bus Radio System

Completed items

Bus Radio Installation

- From fleet of 5,820 buses approximately 50 remaining to be retrofitted, which will be completed when they are delivered.
 - 907 URLC (Universal Radio Logic Controllers) upgrade and, 20 Emergency Alarm switch replacement will be completed as part of punch list.

Radio Base Stations

- All 35 Base Stations needed for the project are in service and transmitting.

Portable Radios

- 1250 portable radios have been delivered and are in full in use by DOB operation staff.

Bus Command Center (BCC)

- Fully completed and operational – Entire fleet is being dispatched from new BCC.

Bus Radio System

Schedule

- ▣ Substantial Completion was declared by C&D on December 31, 2024.
- ▣ C&D plans to close out the contract by December 2025.

Budget

- ▣ The current budget is \$338M and EAC is \$356M.
 - ▣ The budget will be revised after resolution of Commercial issues are negotiated.
- ▣ Payment is being held by C&D for the Contractor to complete punch list items.

Bus Radio System

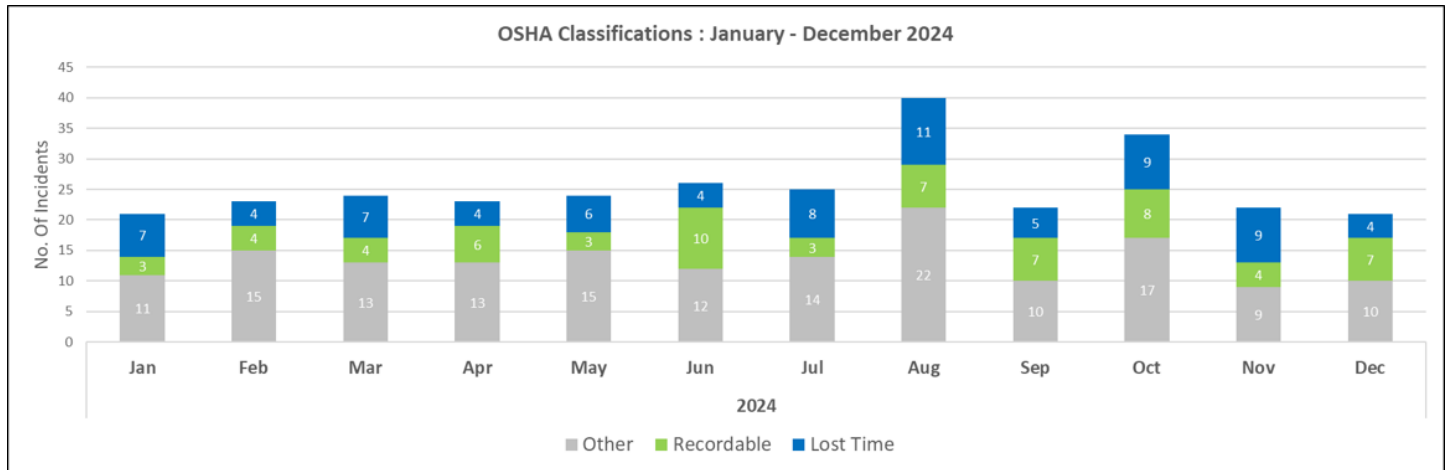
Top Risks and Mitigations

- Risk – Delay in final resolution of Commercial issue may impact contract close out date.
 - Mitigation: Mediation is in progress.
- Risk – Delay in upgrading the remaining 907 URLC within 90 days punch list closure period.
 - Mitigation
 - C&D to hold 2 times the value of remaining work per contract.
 - All URLCs are performing well and are not impacting BRS operation.
- In the IEC’s opinion, above measures are effective in mitigating the risks identified.
- Risks identified by IEC in last year report have been resolved by C&D and the Contractor.

Bus Radio System

Observation

- BRS is substantially complete, fully operational and it is in full beneficial use.
- IEC team visited Brooklyn Bus Command Center (BCC) in mid-November 2024 and received full tour and demonstration of BRS by DOB and C&D representatives and witnessed BRS is fully operational with DOB satisfaction.
- To address AT&T interference issue, the Contractor has transitioned primary communication to 800 MHz from 700 MHz where needed, which has resolved earlier interference issue with positive results.
- Before completion of one year warranty after Substantial Completion date, NYCT per contract has the option to exercise a separate 5-year contract with the Contractor to continue maintaining BRS using operating fund.



Lost Time – A work-related incident (injury or illness) to an employee that results in a loss of productive work time, and the employee is unable to perform regular job duties.
Recordable - An injury or illness that results in restricted work or transfer to another job, medical treatment beyond first aid, or a loss of consciousness.
 Other: A combination of minor first-aid, medical events, and incidents notification-only.

SAFETY NARRATIVE

DECEMBER UPDATE:

- 21 safety incidents were reported in December 2024, including:
 - Four (4) lost time incidents,
 - Seven (7) recordable incidents.
- The reported lost time incidents in December 2024 decreased by five incidents compared to November 2024.
- The Leading lost-time and recordable incident types for December 2024 were Struck by/Against (64%) and Caught Between (27%).
- **SERIOUS INCIDENTS: None**

YEAR-TO-DATE TRENDS:

- **LOST TIME INCIDENT TRENDS:** 78 Lost Time incidents have been reported YTD (through December 31, 2024). The top injury types associated with lost time incidents YTD are Struck By/Against, currently at (40%) Slips, Trips and Falls (27%), and Strains/Sprains (23%). There was a five-incident decrease in reported Lost Time incidents from the previous month.
- **RECORDABLE INCIDENT TRENDS:** 66 Recordable incidents have been reported YTD (through December 31, 2024). The top injury types associated with recordable incidents YTD are Struck By/Against, currently at (56%) & Caught In Between (20%), and Slips, Trips, and Falls (17%). There was an increase of three incidents in the reported recordable incidents from the previous month.
- **SERIOUS INCIDENTS: None**

INSPECTIONS & AUDITS: Active Capital Projects for December – **207 Projects with 382 Sub-Projects**

- **DECEMBER INSPECTIONS:**
 - **INTERNAL – 293**
 - **EXTERNAL – 763** (64 Third-Party Safety Consultants; 699 OCIP Visits)
- **YTD TOTAL # OF INSPECTIONS:**
 - **INTERNAL – 3,204**
 - **EXTERNAL – 8,798** (827 Third-Party Safety Consultants; 7,971 OCIP Visits)
- **DECEMBER NEGATIVE OBSERVATION(S)** – Negative Findings identified through various inspections include General Safety/Housekeeping, Fire Protection/Prevention, Stairs/Ladders, Fall Protection, Electrical, Maintenance and Protection of Traffic (MPT), Supervision/Organization, Industrial Hygiene.
- **DECEMBER POSITIVE OBSERVATION(S)**—Positive Findings identified through various inspections include General Safety/Housekeeping, Fire Protection/Prevention, Fall Protection, Supervision/Organization, Industrial Hygiene, Stairs/Ladders, Electrical, Maintenance, and traffic Protection (MPT).

- **INVESTIGATIONS:**

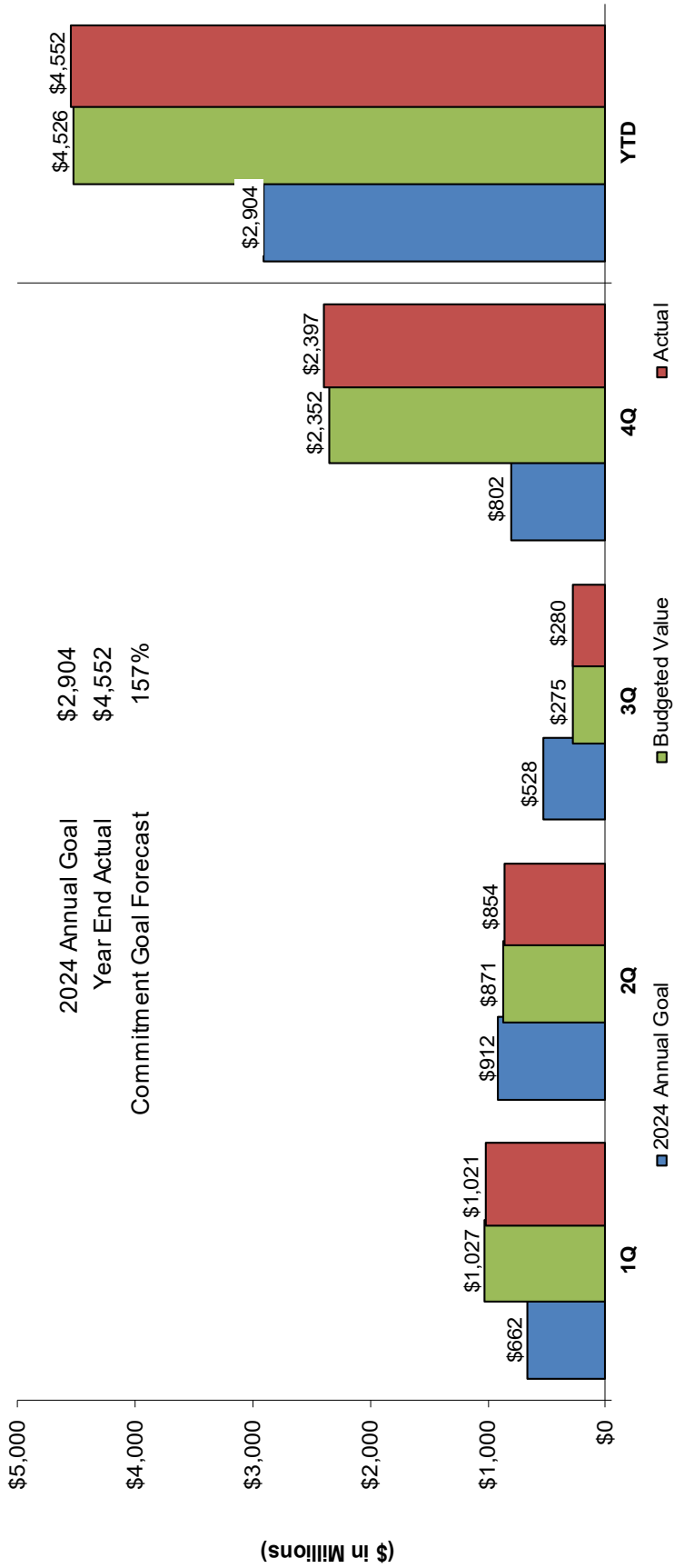
- Number of investigations for DECEMBER– None

- **MTA C&D STRATEGIC INITIATIVES:**

- C&D Safety has successfully embraced the mobile safety inspection application from our OCIP Safety program vendor. We are committed to addressing all concerns raised by our dedicated field and office users in partnership with the vendor. Our pursuit of excellence shines through in our bi-weekly meetings with various safety teams, where we diligently review the platform's performance and explore necessary updates and enhancements.
 - To empower our users, we have crafted an engaging video training module for PCEOs and other personnel, ensuring that they are ready to utilize the system effectively. A significant enhancement we've introduced is the ability for PCEOs to designate team members to tackle any open corrective actions. Furthermore, our safety team provides comprehensive training for PCEOs and their delegates, guiding them in making the most of the system. We are steadfast in our mission to create an effective and user-friendly platform.
- C&D is actively engaging in constructive dialogue with the Building Trades Employers' Association (BTEA) and other important organizations, including the General Contractors Association (GCA), to assess C&D trends and explore potential collaboration opportunities thoughtfully.
- C&D Emergency Management is enthusiastically collaborating with the C&D project team and the agency's Emergency Management team to finalize the next Tabletop Exercise, scheduled for January 2025. Together, we are committed to building a resilient emergency management system that will empower MTA stakeholders, enhance coordination, and inspire more efficient operations.
- C&D is excited to work with Corporate Quality to address the items identified in the ISO 45001 pre-audit as we progress toward certification. Our team is fully committed to tackling the remaining tasks and resolving any minor findings. Together, we're striving to achieve full certification for the corporate safety program by the end of Q4'25 or early Q1'26.
- C&D is collaborating with AECOM to finalize the rollout phase, ensuring our practices align with current contracts. We are updating the Safety Management System (SMS) and refining safety processes. C&D Legal is revising the Division 1 contract to explicitly include new safety programs. Once implemented, all new contracts will comply with the updated SMS and its initiatives, aimed at enhancing our safety practices and culture.
- C&D is enhancing staff safety training by updating policies to align with the new Safety Management System (SMS) and OSHA standards, including Site Safety Training (SST). The Safety Department aims to foster safety awareness among MTA employees and project personnel while implementing improved procedures for employee health and safety, such as hearing conservation and respiratory protection measures.
- C&D Security continues a collaborative initiative with all MTA Security departments to establish streamlined processes for every Construction and Development (C&D) contract and MTA operating agency. We are committed to developing and enhancing robust security procedures tailored specifically for C&D staff and projects, ensuring a safer and more efficient future for all.
- C&D Safety, in partnership with the Delivery Service Office (DSO), has made significant progress by adding a safety reporting portal to the C&D Project Performance Portal. This enhancement provides users with valuable, real-time insights into reported incidents across various C&D projects. Looking ahead, we are excited to introduce even more features in future updates, including incident rates, project labor hour reports, site visit observations and ratings, as well as the integration of safety data from both internal and external sources. These updates will further strengthen our commitment to safety and continuous improvement.
- C&D is committed to proactive safety communication with our staff through the established monthly Safety Zone newsletter, the quarterly Safety Digest, and interactive Safety Moment presentations. This initiative plays a significant role in ensuring that our team is well-informed about important safety information and best practices, while also promoting a collaborative culture of safety throughout the organization and striving for continuous improvement in our safety culture.

**MTA Capital Program
Commitments & Completions
through
December 31, 2024**

Capital Projects – Commitments – December 2024



Annual Goals: Dollar and time-based programmatic milestones for the commitment of contracts established at the start of each year and which are achievable during the year.

Actuals: The value of the goals and any additional unplanned commitments as they are achieved during the year.

Budget: The budgeted value assumed in the capital program for the Actual and Forecasted commitments tracked during the year.

Commitments Summary

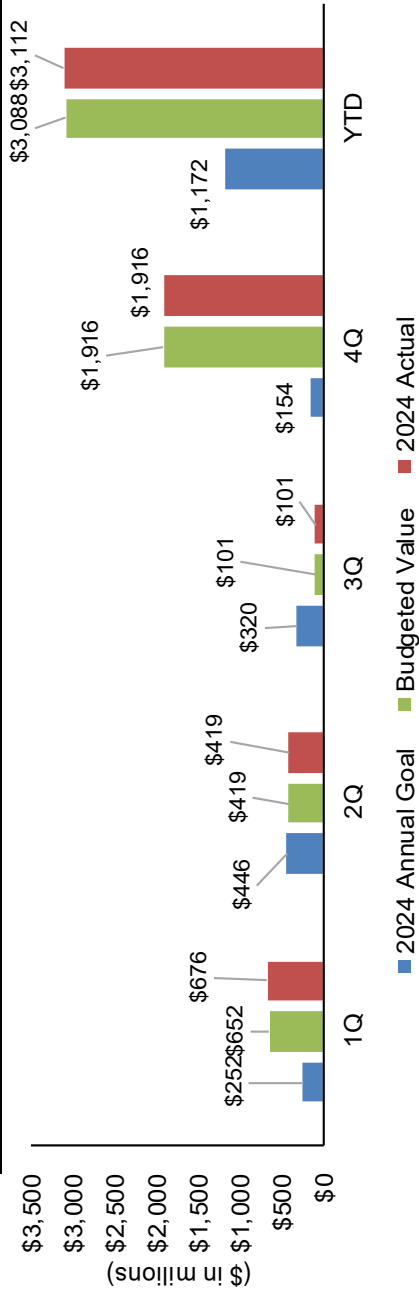
Due to the impact of delayed congestion pricing funding the MTA planned to commit \$2,904 million worth of capital projects in 2024. Through December, the MTA committed \$4,552 million, exceeding its 2024 goal by 57%.

The better-than-anticipated performance is primarily a result of awarding several projects that were not part of the initial \$2.9 billion plan. These awards include 225 battery electric buses (\$410 million); ADA Package #6 (\$422 million); Painting for Portal to Kings Hwy/Culver (\$139 million); Paint/Structure Repair: 225 Street - 242 Street/Bwy-7th Ave (\$131 million); Battery Electric Bus Charging Phase 2 at 6 depots (\$196 million); the phase 2 option for additional repair work to Metro-North's Park Avenue Viaduct (\$241 million); and the LIRR's initial portion of a design-build project to address Superstorm Sandy related repairs at the East River Tunnel (\$99 million).

NYCT/MTA Bus Capital Projects – Commitments – December 2024 – Budget Analysis and Schedule Variances

NYCT and MTA Bus Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$252	\$446	\$320	\$154	\$1,172
2024 Actual	\$676	\$419	\$101	\$1,916	\$3,112
Budgeted Value	\$652	\$419	\$101	\$1,916	\$3,088



Schedule Variances

Project **Commitment** **Goal** **Act./Forec.**

3 NYCT/MTA Bus Red Commitments

Red delays are beyond 2 months of goal.

Line Structures

Line Structure Component Repair:	Construction	Apr-24	Dec-24 (A)
BW7 & 8AV North		\$266.0	\$182.2
Change in aw ard date reflects the extension of bid due date to address bidders' questions and a current extension to qualify the apparent low bidder.			

Stations

2020-2024 Facility Elevators	Construction	Apr-24	Feb-25
		\$43.6	\$35.3

Project was delayed due to the unavailability of Congestion Pricing funding. Current aw ard date, including a delayed aw ard from 12/2024 to early 2025, reflects latest procurement schedule.

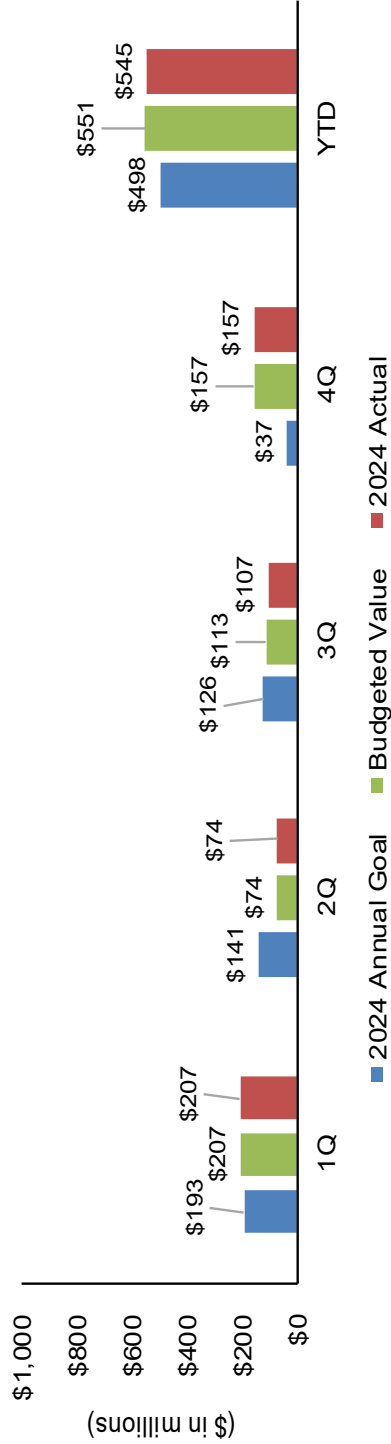
Signals

Equip B Division with Second CBTC	Construction	Jul-24	Mar-25
Radio		\$200.0	\$184.6
Change in aw ard date, including a delayed aw ard from 12/2024 to early 2025, reflects the latest procurement schedule. Change in cost reflects RTA estimate.			

LIRR Capital Projects – Commitments – December 2024 – Budget Analysis and Schedule Variances

LIRR Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$193	\$141	\$126	\$37	\$498
2024 Actual	\$207	\$74	\$107	\$157	\$545
Budgeted Value	\$207	\$74	\$113	\$157	\$551



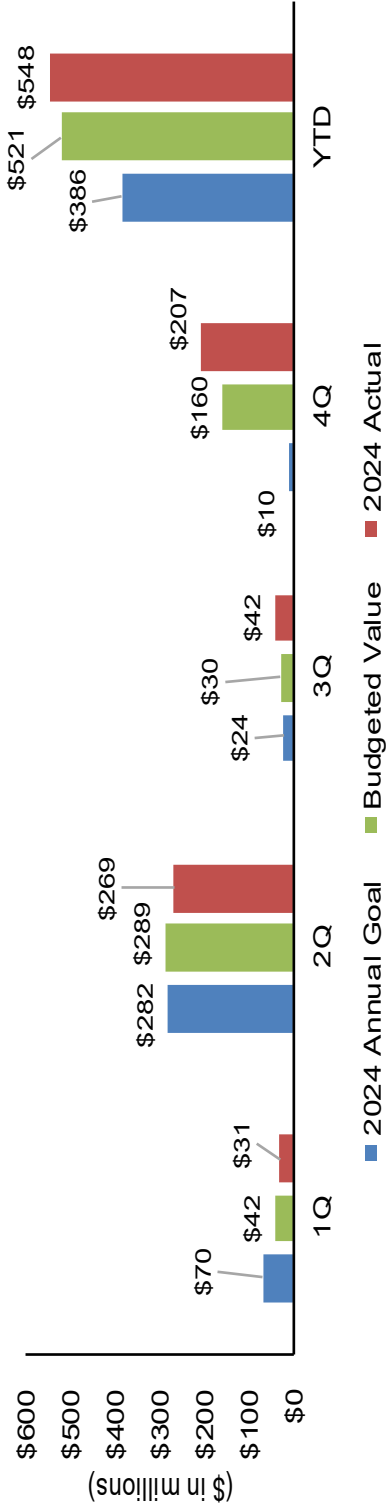
Schedule Variances

Project	Commitment	Goal	Act./Forec.
1 LIRR Amber Commitment			
Amber delays are within 2 months of goal.			
Stations			
Brookhaven National Lab Station (East Yaphank Station)	Construction	Jul-24 \$12.5	Aug-24 (A) \$6.7
Initial low bidder retracted their bid and the award had to be negotiated with the next lowest bidder. The bid was less than budget.			
Yards			
West Side Yard Flood Wall	Construction	Jun-24 \$106.0	Aug-25 \$106.0
The proposal due date for this RFP was extended to 2025.			
Rolling Stock			
Dual Mode Locomotives	Purchase	Jul-24 \$93.3	Feb-25 \$93.3
Currently in active negotiations with Siemens for an LIRR Option, pending funding availability.			

MNR Capital Projects – Commitments – December 2024 – Budget Analysis and Schedule Variances

MNR Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$70	\$282	\$24	\$10	\$386
2024 Actual	\$31	\$269	\$42	\$207	\$548
Budgeted Value	\$42	\$289	\$30	\$160	\$521



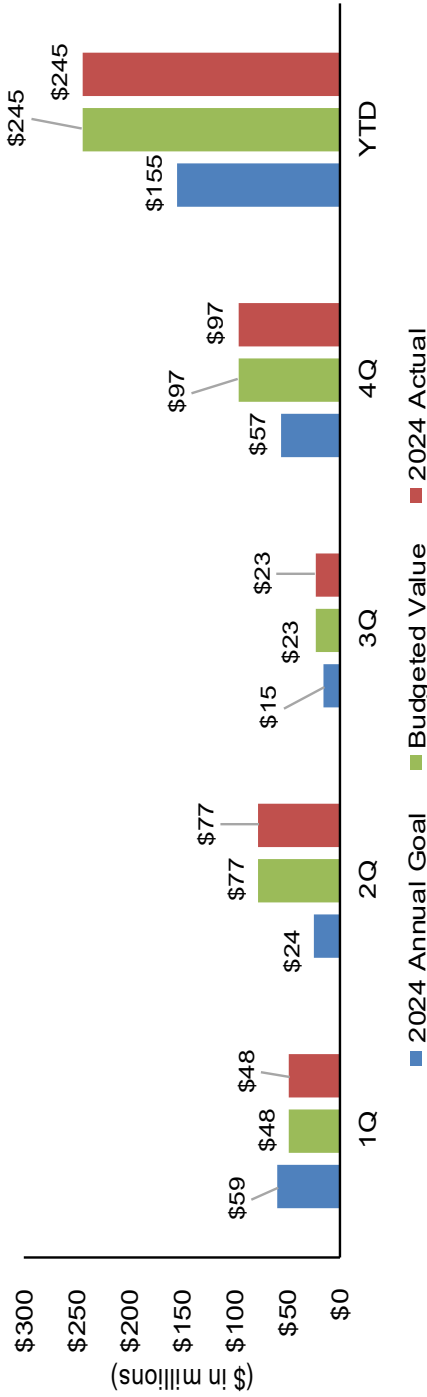
Schedule Variances

There are no major schedule slippages to report for MNR.

MTA Network Expansion Projects – Commitments – December 2024 – Budget Analysis and Schedule Variances

MTA Network Expansion Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$59	\$24	\$15	\$57	\$155
2024 Actual	\$48	\$77	\$23	\$97	\$245
Budgeted Value	\$48	\$77	\$23	\$97	\$245



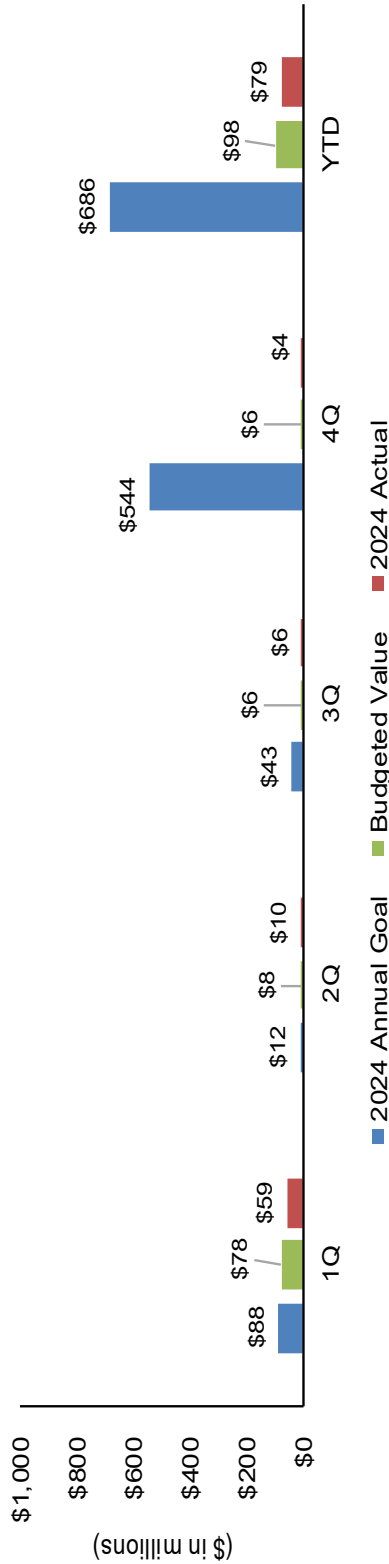
Schedule Variances

There are no major schedule slippages to report for Expansion.

B&T Capital Projects – Commitments – December 2024 – Budget Analysis and Schedule Variances

B&T Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$88	\$12	\$43	\$544	\$686
2024 Actual	\$59	\$10	\$6	\$4	\$79
Budgeted Value	\$78	\$8	\$6	\$6	\$98



Schedule Variances

Project	Commitment	Goal	Act./Forec.
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2 B&T Red Commitments (1 new this month)

Red delays are beyond 2 months of goal.

Widening of SB FDR Drive at the RFK Bridge	Construction	Aug-24	Jul-25
		\$28.5	\$28.5

The procurement was on hold due to the availability of funding in 2024. Current award date reflects the latest procurement schedule.

Reconstruct the Upper Level Approaches VZN Br. (New Item)	Construction	Dec-24	Aug-25
		\$516.6	\$516.6

The procurement was on hold due to the availability of funding in 2024. Current award date reflects the latest procurement schedule.

Capital Projects – Completions – December 2024

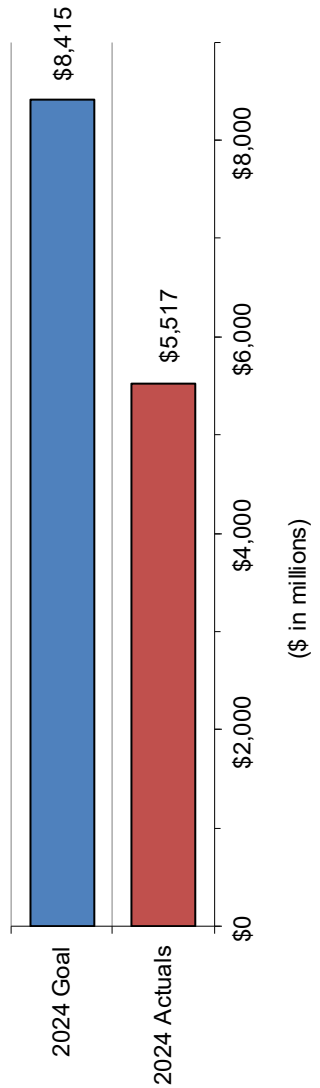
MTA-wide 2024 Major Completions														
Forecast		Jan-24	Feb-24	Mar-24	Apr-24	May-24	Jun-24	Jul-24	Aug-24	Sep-24	Oct-24	Nov-24	Dec-24	Post 2024
Goal	59	3	4	2	2	3	5	3	1	1	1	2	16	16
Jan-24	3	3												
Feb-24	4	3	1											
Mar-24	5	1	2	1										1
Apr-24	6		1	2	3									
May-24	2			1										
Jun-24	5				1									1
Jul-24	2					1					1			
Aug-24	4							1						1
Sep-24	8								1					2
Oct-24	2													2
Nov-24	3											2		1
Dec-24	15					1							7	1

Count Percent Change from Prior Month
 3 5% -
 23 39% ↓ 2
 7 12% -
 26 44% ↑ 2
 59 100% -

BLUE = Actual/Forecast earlier than Goal
 GREEN = Actual/Forecast matches Goal
 AMBER = Actual/Forecast within 2 months of Goal
 RED = Actual/Forecast beyond 2 months of Goal

Budget Analysis

2024 Annual Goal \$8,415
 Year End Actual \$5,517
 Completion Goal Forecast 66%



Completions Summary

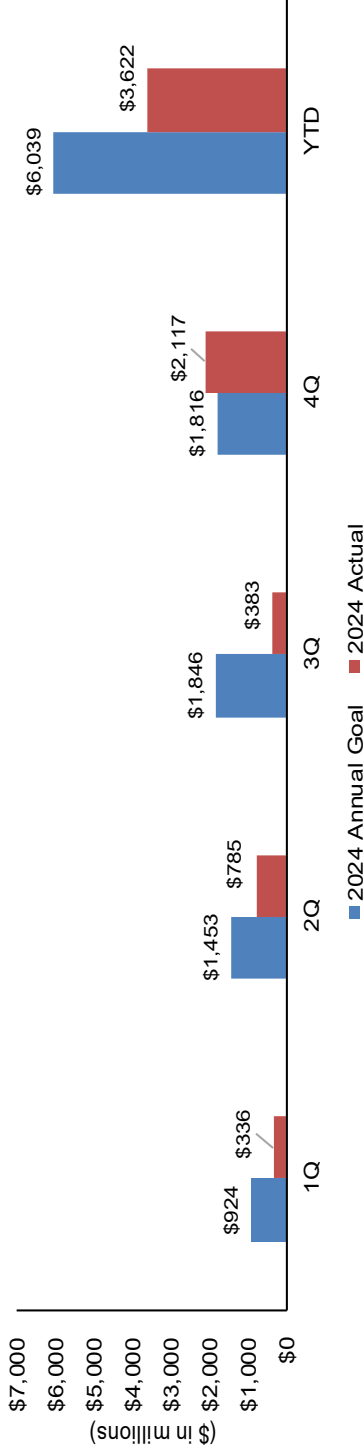
In 2024 the MTA planned to complete \$8.4 billion in projects. 59 major completions were tracked for the year. The MTA completed \$5,517 million versus its goal of \$8,415 million. The shortfall is due to delays in several third-party projects (detailed below), as well as an extension of schedules for in-house capital projects. Delays to major completions include CBTC Culver (\$453 million); Replace 12 Escalators (\$95 million); CBTC Queens Blvd Phase 1 (\$239 million); ADA at 149th Street & Tremont Ave (\$163 million); Sandy Mitigation – Upgrade Emergency Booth Communication Systems (\$86 million); Rehabilitate the Forsyth Street Fan Plant (\$81 million); Platform Components and Track at Broadway 7th Ave (\$72 million); and Regional Investment’s Eastbound Re-route (\$163 million). These delayed completions are forecast for 2025.

NYCT/MTA Bus Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

NYCT and MTA Bus Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$924	\$1,453	\$1,846	\$1,816	\$6,039
2024 Actual	\$336	\$785	\$383	\$2,117	\$3,622

2024 Goal (Rolling Stock)	\$0	\$96	\$89	\$251	\$347
2024 Actual (Rolling Stock)	\$0	\$100	\$0	\$209	\$310



Schedule Variances

Project	Completion	Goal	Act./Forec.
---------	------------	------	-------------

3 NYCT/MTA Bus Amber Completions (1 new this month)

Amber delays are delayed less than 2 months of goal.

Stations ADA			
ADA: 8 Stations (Package A)	Construction	Feb-24	Apr-24 (A)
Change in schedule reflects delay in fire alarm testing due to ConEd power issues at Metropolitan Ave station.		\$278.8	\$270.3
Track			
Direct Fixation Track Replacement:	Construction	Apr-24	Jun-24 (A)
63rd St & Jamaica		\$183.4	\$204.3
The change in project schedule was due to pending as-built drawings.			

MTA Bus

173 Standard Buses - Nova (New item)	Purchase	Dec-24	Feb-25
Through 2024, 158 of 173 buses were delivered with the remaining buses scheduled for acceptance in 2025.		\$149.4	\$113.3

Project	Completion	Goal	Act./Forec.
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16 NYCT/MTA Bus Red Completions (2 new this month)

Stations			
Platform Components & Track / BW7 (New Item)	Construction	Oct-24	Mar-25
The delayed schedule is due to track access coordination adjustments needed for change orders.		\$72.3	\$72.3
Signals			
CBTC QBL Ph. 1 Siemens (New Item)	Construction	Dec-24	Jun-25
Completion has slipped to 06/2025 due to ongoing system reliability and availability issues, ATS softw are delays and fleet reliability and availability issues. Cost increase is due to additional need for TA Labor.		\$226.1	\$239.3

NYCT/MTA Bus Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

Schedule Variances

Project	Completion	Goal	Act./Forec.
16 NYCT/MTA Bus Red Completions (continued)			
Red delays are beyond 2 months of goal.			
<i>Shops & Yards</i>			
207th Street Yard: Sandy Repair/Mitigation	Construction	May-24 \$600.7	Dec-24 (A) \$611.5
The delayed completion was due to field conditions and pending as-built drawings. Change in cost reflects an updated estimate at completion.			
<i>MTA Bus</i>			
Storeroom Expansion - LaGuardia Depot	Construction	Mar-24 \$7.4	Jan-25 \$7.4
Due to the complaint about the area's bird infestation, the storeroom relocation has been put on-hold. The contractor's estimate was much higher than anticipated. Therefore, an alternative solution was proposed and is pending approval.			
25 Standard Buses (2015- 19 capital program)	Purchase	Jun-24 \$16.8	Dec-24 (A) \$16.4
Completion date slipped due to production delays, 110 of the total 135 buses were delivered by end of June. 100% of the vehicles were delivered by year-end.			
25 Standard Buses (2020- 24 capital program)	Purchase	Jun-24 \$17.7	Dec-24 (A) \$17.7
Completion date slipped due to production delays, 110 of the total 135 buses were delivered by end of June. 100% of the vehicles were delivered by year-end.			
85 Standard Buses	Purchase	Jun-24 \$61.9	Dec-24 (A) \$61.9
Completion date slipped due to production delays, 110 of the total 135 buses were delivered by end of June. 100% of the vehicles were delivered by year-end.			
Spring Creek Depot Compressed Natural Gas (CNG) Upgrade	Construction	Jul-24 \$7.0	Oct-24 (A) \$7.0
Completion date adjusted to account for delay in delivery of CNG gas dispensers.			
<i>Communications</i>			
Passenger ID CCTV: Various Locations	Construction	Nov-24 \$65.7	Jun-25 \$65.7
Change in schedule due to ongoing coordination with Department of Subways on system integration.			

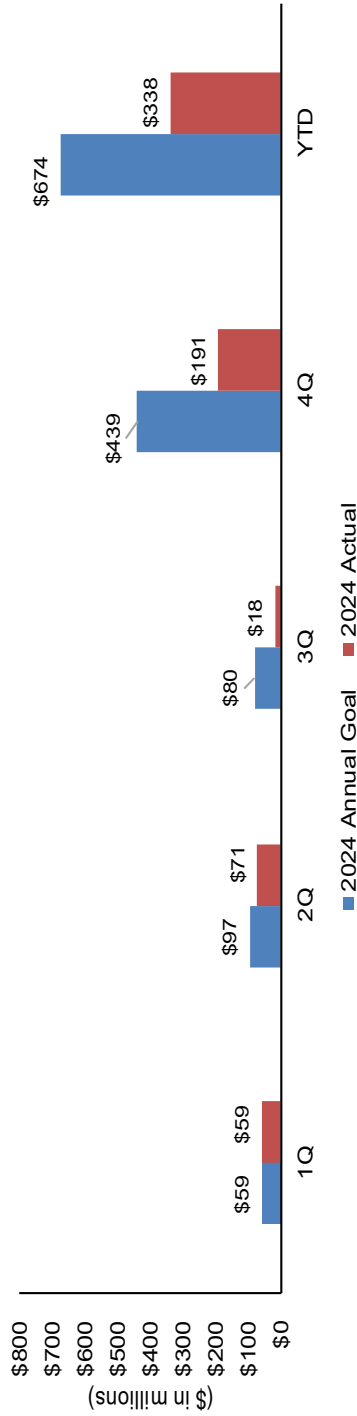
Project	Completion	Goal	Act./Forec.
<i>Signals</i>			
CBTC Culver	Construction	Aug-24 \$452.0	Feb-25 \$453.2
Change in schedule due to delay in the submission and approval of as-builts and Operations and Maintenance (O&M) manuals as well as wayside corrective work.			
<i>Stations</i>			
Replace 12 Escalators	Construction	Sep-24 \$83.8	Apr-25 \$94.9
Change in schedule due to other escalator work at Lexington Ave station and Elevator Maintenance Room (EMR) structural repairs. Change in cost reflects latest EAC.			
ADA Pkg 2: ADA/Elevators at Legacy Sattons	Construction	Sep-24 \$411.2	Dec-24 (A) \$423.4
The delayed completion was due to additional steel defects which needed to be addressed. Change in cost reflects a late year revised estimate at completion.			
ADA: 149 St & Tremont Av	Construction	Dec-24 \$157.5	Mar-25 \$163.0
Change in schedule due to delays in fabrication and installation of street elevator at 149 St-Grand Concourse. Change in cost reflects latest estimate at completion.			
<i>Depots</i>			
Bus Radio System	Construction	Aug-24 \$263.7	Dec-24 (A) \$251.1
Substantial completion date subject to mediation with contractor. Change in cost reflects latest estimate at completion.			
<i>Line Equipment</i>			
Rehabilitate Forsyth St. Fan Plant	Construction	Sep-24 \$80.6	Mar-25 \$80.9
Delay in completion is due to ongoing work to address minor water leaks at fan plant.			
<i>Sandy</i>			
Sandy Mitigation: Upgrade Emergency Booth Comm System	Construction	Dec-24 \$75.8	Aug-25 \$85.9
Change in schedule due to change order to incorporate Elevator 2-Way Communication System (E2CS). Change in cost reflects latest estimate at completion.			

LIRR Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

LIRR Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$59	\$97	\$80	\$439	\$674
2024 Actual	\$59	\$71	\$18	\$191	\$338

2024 Goal (Rolling Stock)	\$0	\$0	\$0	\$0	\$0
2024 Actual (Rolling Stock)	\$0	\$0	\$0	\$0	\$0



Schedule Variances

Project	Completion	Goal	Act./Forec.
4 LIRR Amber Completions			
Amber delays are delayed less than 2 months of goal.			
<i>Stations ADA</i>			
ADA Copiague	Construction	Apr-24 \$18.0	May-24 (A) \$17.4
Delay with materials for the curtain wall and installation of the elevator components.			
ADA Lindenhurst	Construction	Apr-24 \$18.0	Jun-24 (A) \$18.0
Delay with materials for the curtain wall and installation of the elevator components.			
ADA Amityville	Construction	Apr-24 \$15.5	Jun-24 (A) \$15.5
Delay with materials for the curtain wall and installation of the elevator components.			
<i>Shops & Yards</i>			
Mets- Willets EIC Relocation	Construction	Apr-24 \$28.7	May-24 (A) \$19.9
Date moved out due to a delay in force account work.			

Project	Completion	Goal	Actual(A)
7 LIRR Red Completions			
Red delays are beyond 2 months of goal.			
<i>Shops & Yards</i>			
Long Island City Yard Resiliency	Construction	Sep-24 \$26.3	Dec-24 (A) \$26.3
Delay due to unforeseen site conditions and log lead items.			
Long Island City Phase 3 B	Construction	Sep-24 \$15.0	Dec-24 (A) \$15.0
Delay due to unforeseen site conditions and long lead items.			
Long Island City Yard -	Construction	Sep-24 \$2.4	Dec-24 (A) \$2.4
Delay due to unforeseen site conditions and long lead items.			
<i>Power</i>			
Hall & Babylon Signal Power Generator Repl.	Construction	Oct-24 \$19.5	Aug-25 \$19.5
Delay due to coordination with ConEd on final direction for primary and secondary service feeders.			

LIRR Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

Schedule Variances

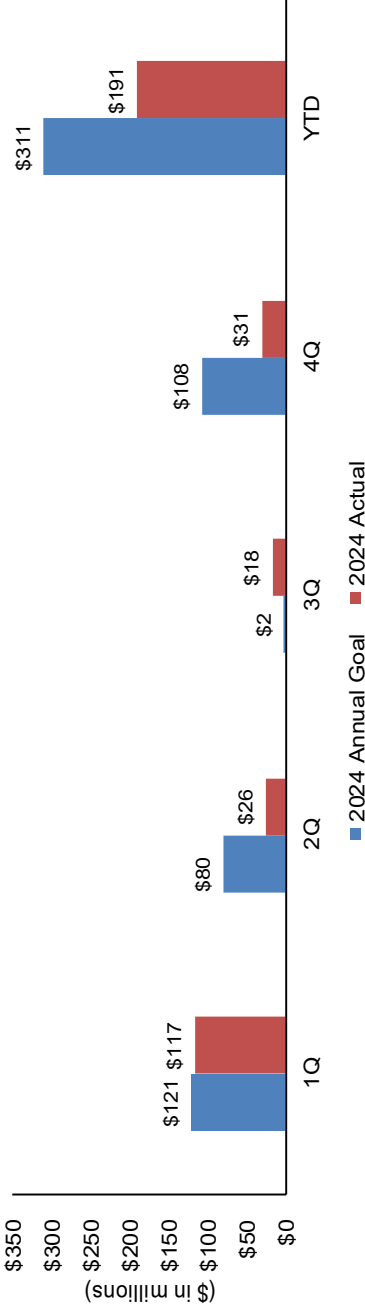
Project	Completion	Goal	Act./Forec.
7 LIRR Red Completions (continued)			
Red delays are beyond 2 months of goal.			
<i>Facilities</i>			
Fire Protection Improvements	Construction	Dec-24 \$25.0	May-25 \$25.0
Completion date extended due to delays with design review, submittal, and safety workplan.			
Rehab of Employee Facilities Bethpage	Constuction	Dec-24 \$24.3	Jul-25 \$24.3
Delay due to HVAC Force Account support work unable to be completed until July 2025.			
<i>Stations</i>			
ADA Accessibility and Components 24 Stations DES	Construction	Dec-24 \$19.8	Sep-25 \$19.8
The design schedule was extended to include the scope of additional stations: Bellerose, Bellmore and Mineola.			

MNR Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

MNR Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$121	\$80	\$2	\$108	\$311
2024 Actual	\$117	\$26	\$18	\$31	\$191

2024 Goal (Rolling Stock)	\$0	\$0	\$0	\$0	\$0
2024 Actual (Rolling Stock)	\$0	\$0	\$0	\$0	\$0



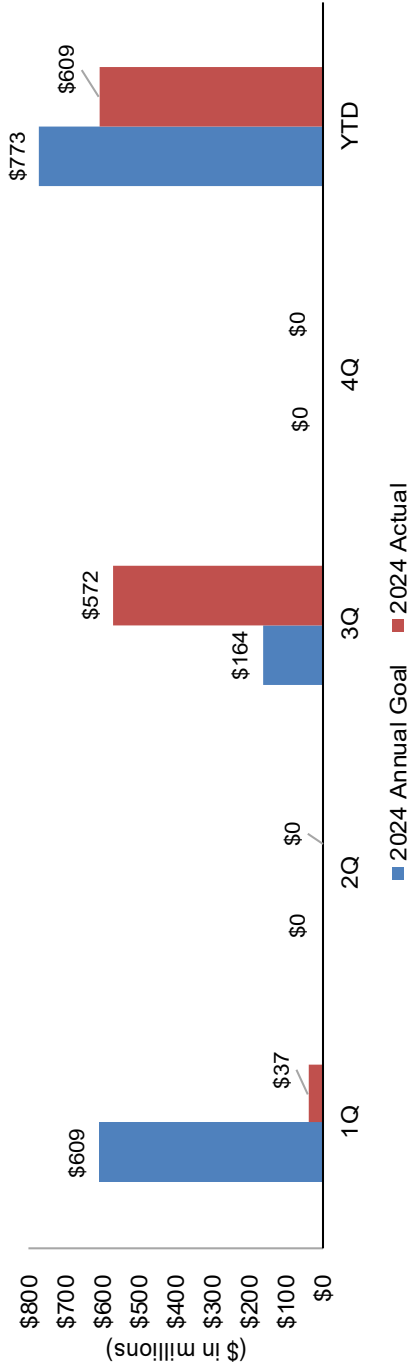
Schedule Variances

Project	Completion	Goal	Act./Forec.
1 Metro-North Red Completion			
Red delays are beyond 2 months of goal.			
Power			
Harlem & Hudson Power Improvements (City Water Substation)	Construction	Jun-24	Jan-25
		\$23.7	\$23.7
Problems stemming from equipment performance issues. Substantial Completion is scheduled for January pending final testing.			

MTA Network Expansion Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

MTA Network Expansion Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$609	\$0	\$164	\$0	\$773
2024 Actual	\$37	\$0	\$572	\$0	\$609



Schedule Variances

Project	Completion	Goal	Act./Forec.
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2 Network Expansion Red Completions

Red delays are beyond 2 months of goal.

East Side Access

GCT Concourse & Facilities CM014B	Construction	Mar-24	Jul-24 (A)
		\$572.0	\$572.0

The slip was due to a delay in the installation/certification of seismic monitoring equipment.

East Bound Re-Route

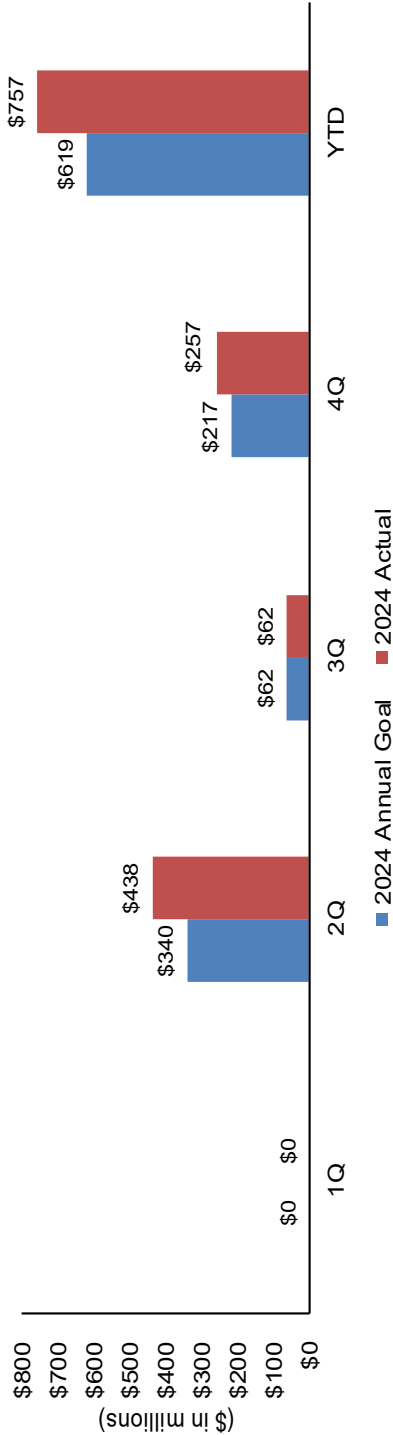
East Bound Re-Route CH058B	Construction	Aug-24	Apr-25
		\$163.6	\$163.6

Ongoing delays continue to be impacted by track outage availability, impacted by Amtrak force account support. The completion date recently slipped from 02/2025 to 04/2025.

B&T Capital Projects – Completions – December 2024 – Budget Analysis and Schedule Variances

B&T Budget Analysis

Summary Chart Data	1Q	2Q	3Q	4Q	YTD
2024 Annual Goal	\$0	\$340	\$62	\$217	\$619
2024 Actual	\$0	\$438	\$62	\$257	\$757

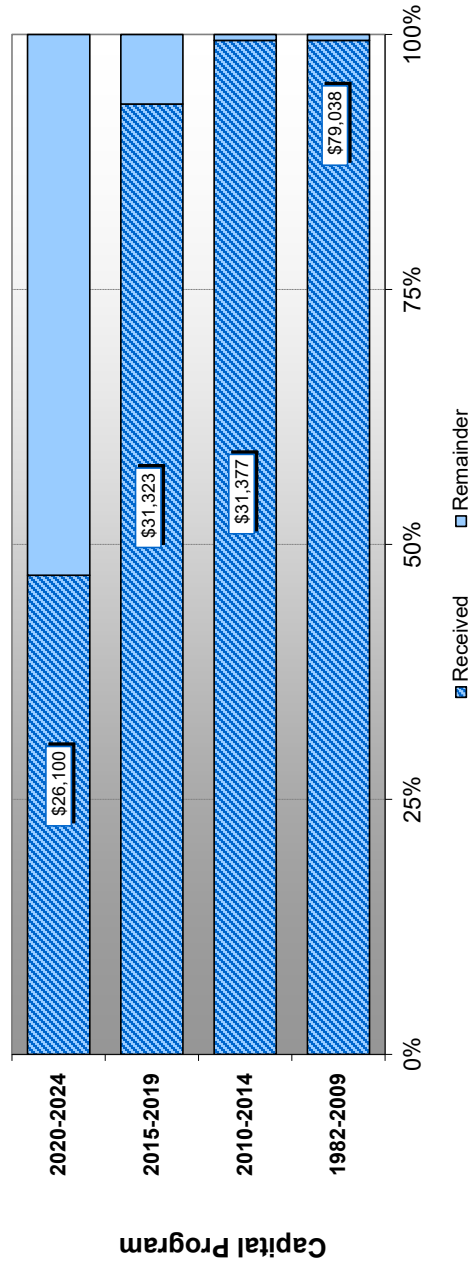


Schedule Variances

There are no major schedule slippages to report for B&T. All projects were completed on or ahead of schedule. In addition, B&T exceeded its completion goal with one major project completion advanced from 2025 into 2024.

Status of MTA Capital Program Funding

Capital Funding (December 2024)
 \$ in millions



Capital Funding Detail (December 2024)

\$ in millions

	Funding Plan		Received/Secured		Secured to date	Remainder
	Current	Thru November	December			
2010-2014 Program						
Federal Formula, Flexible, Misc	\$5,841	\$5,790	\$ -	\$5,790	\$51	
Federal High Speed Rail	173	173	-	173	-	
Federal New Start	1,271	1,271	-	1,271	-	
Federal Security	89	89	-	89	-	
Federal RRIF Loan	-	-	-	-	-	
City Capital Funds	528	524	-	524	4	
City Asset Sales	195	84	-	84	110	
State Assistance	770	770	-	770	-	
MTA Bus Federal and City Match	132	113	-	113	19	
MTA Bonds	11,654	11,635	20	11,654	- a	
Other (Including Operating to Capital)**	1,261	1,290	(28)	1,261	- a	
B&T Bonds	1,975	2,000	(25)	1,975	- a	
Hurricane Sandy Recovery						
Insurance Proceeds/Federal Reimbursement	6,677	6,697	(21)	6,677	0 a	
PAYGO	18	18	(0)	18	- a	
Sandy Recovery MTA Bonds	658	658	(0)	658	- a	
Sandy Recovery B&T Bonds & Cash	318	383	(64)	318	- a	
Total	31,561	31,495	(119)	31,377	184	1%

	Funding Plan		Received/Secured		Secured to date	Remainder
	Current	Thru November	December			
2015-2019 P Program						
Federal Formula	\$4,706	\$4,877	(\$170)	\$4,706	\$ - a	
Federal Flex & Other (Incl HSR/Security/Core Capacity)	649	628	-	628	20	
Federal New Start	1,400	1,400	-	1,400	-	
State Assistance	9,118	8,248	-	8,248	871	
City Capital Funds	2,092	2,066	-	2,066	27	
City Non-Tax Levy Revenue Sources	600	-	-	-	600	
MTA Bonds	9,098	9,118	(20)	9,098	- a	
Asset Sales/Leases	906	326	-	326	581	
Pay-as-you-go (PAYGO)**	2,105	2,145	(40)	2,105	- a	
Other	267	70	-	70	197	
B&T Bonds & PAYGO/Asset Sale	2,677	2,942	(265)	2,677	- a	
Total	33,619	31,819	(495)	31,323	2,295	7%

	Funding Plan		Received/Secured		Secured to date	Remainder
	Current	Thru November	December			
2020-2024 Program						
Capital from Central Business District Tolling	\$15,000	\$ -	\$ -	\$ -	\$15,000	
Capital from New Revenue Sources	10,000	5,604	-	5,604	4,396	
MTA Bonds and PAYGO	7,385	723	-	723	6,662	
Other Contribution	589	-	-	-	589	
Federal Formula	9,921	10,445	170	10,615	(694) a	
State of New York	3,169	811	-	811	2,358	
City of New York	3,007	3,031	-	3,031	(24)	
Federal New Start (SAS Ph2)	2,005	2,005	-	2,005	-	
Federal Flexible & Other	1,161	1,148	1,415	2,563	(1,402) b	
B&T Bonds	3,327	748	-	748	2,579	
Total	55,563	24,515	1,586	26,100	29,463	53%

a) The 2010-2014, 2015-2019 and 2020-2024 Capital Programs amendments were approved in December 2024. Capital funding plans and receipts in this report are adjusted accordingly.
 b) Receipt of \$1,415B in FRA grant funds for Penn Station Access. With the award of the FRA grant, Amtrak's original contribution to this project totaling \$500m was offset by -\$365m.